

PLANNING COMMISSION STAFF REPORT

Salt City Plaza **PLNPCM2009-00042**

Conditional Use/Planned Development:
154 W. 600 South and 179 W. 500 South
April 22, 2009



Planning and Zoning Division
Department of Community and
Economic Development

Applicant:

Salt City Plaza LLC

Staff:

Doug Dansie, 535-6182
Doug.Dansie@slcgov.com

Tax ID:

15-01-476-001; 002; 014; 017

Current Zone:

D-1 Central Business District:

Master Plan Designation:

Downtown Master Plan: Central
Business District

Council District:

District Four Luke Garrott

Lot Size:

4.92 acres

Current Use:

Hotel

Applicable Land Use**Regulations:**

21A.30.020 D-1 Central
Business District:
21A.54 Conditional Uses

Notification

- Notice: April 7, 2009
- Sign: April 11, 2009
- Web: April 17, 2009

Attachments:

- A. Site Plan & Elevation
Drawings.
- B. Photographs
- C. Conditional use map
- D. PC Subcommittee notes

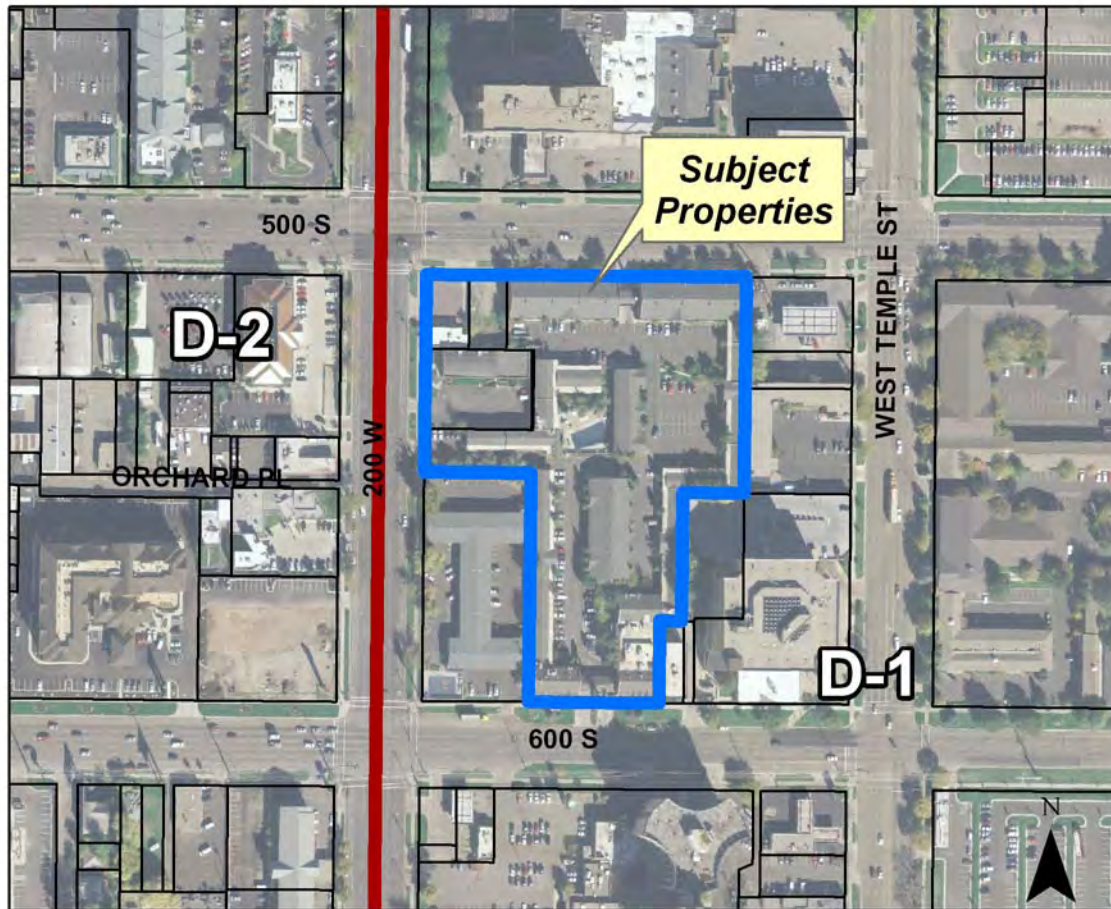
Request

This is a request from Salt City Plaza, LLC for a Planned Development located at approximately 154 W. 600 South and 179 W. 500 South. The site is presently zoned D-1 Central Business District. The petitioner is proposing to construct multiple building on the site. Multiple buildings on a single site without street frontage for all buildings, requires planned development approval. The petitioner is also requesting a conditional use for some design issues such as height and setback.

Staff Recommendation

Based on the findings listed in the staff report, it is the Planning Staff's opinion that overall the project generally meets the applicable standards and therefore, recommends that the Planning Commission approve the petition, with the condition that it meet all other City Code requirements, including compliance with the landscaping, screening, pedestrian access and Street Lighting plan. Exceptions to individual urban design criteria (maximum setback and door facing the street) shown on the plan are accepted as noted in the staff report. Alterations to the public way (200 West) implied on some plans, are not accepted.

The Planning Commission delegates final authority for the site plan, and landscaping to the Planning Director.



VICINITY MAP

Background

Project Description

The proposal is to construct a hotel /office/retail project between 500 and 600 South and West Temple and 200 West. There is presently a motel on the site. The northern portion of the existing motel will be saved and used, however the remainder of the site will be replaced with underground parking and three new separate hotels. A new office building will be built on the corner of 200 West and 500 South. A retail pad is also planned for 200 West. The site has frontage on 500 South, 600 South and 200 West. The proposal is for multiple buildings on the site. Multiple buildings on a single site without street frontage for all buildings requires planned development approval. Two of the hotel buildings would front 600 South and the third hotel building is proposed at the center of the block, without street frontage. Two hotel buildings are perpendicular to the road; however, the portions of the buildings facing 600 South are predominantly glass so that they do not turn their sides to the street. The property is bisected by a central vehicular aisleway that focuses on the third hotel building.

The main entry to the complex will be from 600 South. The number of curb cuts on 600 South is limited by the fact that the road is a state highway. Parking entry will be from 200 West.

The office building will occupy the corner of 500 South and 200 West, with additional retail space south of the office building on 200 West. The petitioners spoke with city staff early in the design of the project and have responded by anchoring the complex with the tallest building on the corner.

Architectural renderings of the buildings are included in the attachments. Preliminary indications are that portions of the building exceed the maximum 5 foot setback as required by the D-1 zoning district (the Planning Commission has the authority to modify this requirement). Not all buildings have doors facing the public street.

Front yard landscaping is being provided according to code.

The City has a Street Lighting Master Plan and as properties are developed, they are generally brought into compliance with this plan. This project is expected to provide public street lighting consistent with the Street Lighting Master Plan.

The private roadway also functions as a mid-block walkway; consistent with the D-1 zoning district

Comments

Public Comments

The Downtown Community Council was contacted on Feb. 19, 26 and March 19, 2009 and again on April 8, and 9, 2009. The Council did not provide input prior to the staff report being posted. The Peoples Freeway Community Council stated it was outside their boundaries and they deferred to the Downtown Community Council

City Department Comments

Airport (David Miller)

Thank you for the notice regarding a Planned Development located at 154 W. 600 South and 179 W. 500 South. This address is not in an established Salt Lake City airport influence zone. The project does not create any observed impacts to airport operations.

Fire

No Comment

Police (Lt Rich Brede)

No issues as long as safeguards are in place for underground parking (specifically lighting).

.Public Utilities (Jason Brown, PE; Justin D. Stoker, PE)

Public Utilities' only comment on the multiple structures on one lot is that all the buildings must be served by a single culinary meter. Below are additional comments for the future development;

All design and construction must conform to State, County, City and Public Utilities standards and ordinances. Design and construction must conform to Salt Lake City Public Utilities General Notes.

Plans must be submitted to our office showing all existing and proposed water, sewer and storm drain pipes and connections points. The plans must show all proposed pipe routings, sizes, types, boxes, meters, detector checks, fire lines and hydrant locations. Culinary and fire connection must be separate connections at the main. For all culinary services larger than 3-inches, the water meter size must be justified by submitting AWWA M-22 method calculations or by an approved equivalent method. The engineer must provide calculations for expected peak sewer flows from this development. With this information Public Utilities will verify if the sanitary sewer system downstream for this development can handle these additional flows. If not, the developer will be responsible to provide offsite improvements as necessary to accommodate these additional flows. All existing water service not used must be killed at the main and all existing sewer services that are not used must capped at the property line per Salt Lake City Public Utilities standards.

A grading and drainage plan must be submitted for this development. Storm water flows are not allowed to sheet flow onto adjacent lots. The development will be required to provide on-site detention of the storm water. High groundwater is typical in this area. If below grade buildings or structures are proposed, a stamped geotechnical report identifying the highest expected groundwater must be submitted to Public Utilities for review and approval. This assessment must be based upon historical well records, borings, etc. All finished floor elevations must be above the highest expected groundwater elevation.

Fire Department approval will be required. Fire flow requirements, hydrant spacing and access issues will need to be resolved with the Fire Department.

All existing and proposed easements must be provided before final plat recordation. If an existing sewer lateral or a water lateral service crosses through an adjacent property, an easement for that utility must be provided.

All sewer, water and storm drain connection agreements must be completed and fees paid in full prior to any approvals from our Department. A \$343 per quarter acre drainage fee will be assessed to this property. If offsite improvements are required, all construction must be bonded for by the developer.

Please call Peggy Garcia or myself if you have any questions.

The Department of Public Utilities has already met with the project engineer, has addressed all department concerns, and has approved civil engineering improvement plans for the subject property. SLCPU is prepared to issue permits for the construction of this project.

There are no additional concerns, from what has previously been addressed.

Permits (Ken Brown)

Re: PLNPCM2009-00042 – Salt City Plaza Planned Development 154 West 600 South and 179 West 500 South

The Building Services Department review comments and recommendations are as follows:

- 1) A separate demolition permit will need to be obtained for each address before removing any structures.
- 2) Because this proposal involves the use of adjacent parcels, these parcels should probably be combined together as one parcel with one parcel number. Building code would generally not allow the Office Building to be built across a property line as shown.
- 3) It is understood that minimum building height and maximum setbacks are being modified through this process. It is also understood that this conditional use process will evaluate any urban design issues related to the down town area.
- 4) This proposal should include the following information and supporting documentation;
 - a) The adjusted parcel/sidwell number and legal description,
 - b) The property lines along with the dimensions of each,
 - c) All public way improvements such as sidewalks, lights, park way strip trees (one tree for each thirty feet of lot width along each frontage), approaches, fire hydrants, etc., and identified as “existing” or “proposed”,
 - d) Parking and bike rack calculations, along with the bike rack location,
 - e) Carpool parking per 21A.44.040B if applicable,
 - f) Dimensions used for determining parking lot area and interior parking lot landscaping areas to verify compliance with the parking lot landscaping requirements,
 - g) The identification of all power and/or utility easements, and any overhead power lines,
 - h) The location and size of all ground mounted utility boxes planned, to determine whether a Routine and Uncontested Matter needs to be processed, and
 - i) Any public way encroachments.

Transportation (Barry Walsh)

Re: PLNPCM2009-00042 – Planned Development: Salt City Plaza at 154 West 600 South and 179 West 500 South and 517 South 200 West.

The division of transportation review comments and recommendations are as follows:

The proposed concept is subject to UDOT review per the Traffic Impact study submittal for 500 South and 600 South roadways. The 200 West roadway proposal for new driveways and cut back angle parking are subject to detailed reviews per Salt Lake City design standards and impacts.

The proposed cut back parking on 200 West indicates eight existing parallel stalls and the conversion to sixteen angle stalls. Per the submitted parking calculation: 547 stalls will be required and 686 stalls provided. The eight added on street public parking stalls does not seem justified. We are concerned with the existing street scape and function being changed per various impacts.

1 – 200 West has bike lanes that would be impacted by angle parking as a safety issue.

2 – As a Hotel, Business, Retail development center, we expect added bus service that may be required to stage along this frontage of 200 West, as is the function in other immediate areas.

3 - The angle parking proposed may also be a visual issue with the major driveway to the new parking structure.

The packet submitted has inconsistencies with street scape, driveway widths, circulation (one ways), drawing scale (parking garage 1"=50'?), etc. for a detailed review at this time.

Fully detailed scale drawings will need to be submitted for reviewed in order to address city design standards, but the over all concept plans are in keeping with development abutting two major arterial roadways – 500 and 600 South and a special collector – 200 West.

We look forward the reviewing the 200 West frontage proposal for; driveway widths, separation, and locations. Pedestrian walks, park strip, street lighting, and curb functions. Along with the proposed parking structures: for column spacing, parking layout, ramp grades and transitions, height clearances for ADA and service access, etc.

Engineering (Randy Drummond, P.E)

Engineering review comments are as follows:

1. This submission consists of an existing mixed use office of an existing hotel, 3 new hotels, underground parking, a new office building, and a new retail building. There are two existing lots within this Planned Development. 500 South, 600 South, and 200 West Streets are fully improved, and have all the required right-of-way. There are numerous sidewalk panels, driveways and curb and gutter sections that must be replaced or repaired prior to receiving final approval from our Division. They are as follows:

500 South: 9 panels of sidewalk have a raised edge, which presents a trip hazard and must be ground down or have the panel replaced. Inasmuch as 500 South is a UDOT road, the street improvements to back of curb line are under the jurisdiction of UDOT. Thus, the developer will need to obtain a permit from UDOT allowing the proposed drive approach. Also, there are two sections of curb and gutter that are severely cracked or sunken that we recommend be replaced. All of the sidewalk repairs and or replacement requirements meet the criteria for replacement as per APWA Std. Dwg. #291. During the site visit, we noticed that the existing sidewalk on 500 South was covered with mud and debris from a utility installation. When the sidewalk is cleaned sufficient to allow inspection, additional panels of sidewalk may need to be replaced.

600 South: Inasmuch as 600 South is a UDOT road, the street improvements are under the jurisdiction of UDOT, and the developer will need to obtain a permit from UDOT allowing the proposed replacement of the drive approaches from two to one access point. We noticed that there are 8 sections of curb and gutter that are sunken or spalled, and the developer will need to find out from UDOT if they will need to be replaced as a condition of permit issuance. There are 3 panels of sidewalk (City jurisdiction) that have a trip hazard, and meet the criteria of APWA Std. Plan 291 for repair. If the sidewalk through the new drive approach is not a minimum of 8" thick concrete, it will need to be replaced as a condition of City approval.

200 West: According to the plans we have reviewed, all of the sidewalk and curb and gutter is proposed to be replaced along this frontage with the project. Salt Lake City Transportation has recommended that the curb and gutter remain in the alignment as presently constructed. If the plan is approved as shown, the curb and gutter shall be replaced as per APWA 205A and 251, the sidewalk as per APWA 231, and the drive approaches as per APWA Std. Plan 225. If not, there are two panels of sidewalk with a trip hazard that need to be ground down, and the new drive approach must be constructed as per APWA Std. Plan 225. If the

developer wishes to replace the handicapped access ramp at the corner, it will have to be designed as per APWA Std Plan 235, and the design approved prior to construction.

Any sidewalk to be replaced must be done as per APWA Std. Plan #231. Any curb and gutter to be replaced must be done as per APWA Std. Plans 205A and 251, with the exception of the curb and gutter on 500 and 600 South, which is UDOT jurisdiction, and must meet the requirements of UDOT. The two new drive approaches on 200 West shall be installed as per APWA Std. Plan 225. All of the improvement repair and replacement on all frontages (with the exception of the curb and gutter and drive approaches on 500 and 600 South) must be completed by a licensed, bonded and insured contractor via a Public Way Permit that can be obtained from our office. The Permit must be obtained prior to the project being given final approval by our Division.

Project Review

Planning Commission Subcommittee

The Planning Commission held a Planned Development subcommittee meeting on February 26, 2009. Commissioners agreed that this should be reviewed by the Downtown Community Council and People's Freeway and suggested that the applicant should have a better understanding of pedestrian circulation and lighting on site for the public hearing.

Analysis and Findings

Options

Failure to grant the planned development would require that the petitioner combine the buildings into one structure, which would make it more difficult to maintain frontage onto all streets because the lot is deep and has multiple frontages.

Findings

21A.54.080 B. Specific Standards: A conditional use permit shall be approved unless the evidence presented shows that one (1) or more of the standards set forth in this subsection cannot be met. The Planning Commission, or, in the case of administrative conditional uses, the Planning Director or the Director's designee, may request additional information as may be reasonably needed to determine whether the standards of this subsection can be met.

1. **Master Plan and Zoning Ordinance Compliance:** The proposed conditional use shall be:
 - a. Consistent with any policy set forth in the City-Wide, Community, and Small Area Master plan and future land use map applicable to the site where the conditional use will be located, and
 - b. Allowed by the zone where the conditional use will be located or by another applicable provision of this title.

Finding: The Downtown Master Plan calls for the area south of the Central Business District and along 600 South to be supportive of the Central Business District and of the hospitality industry. This proposal is consistent with the master plan.

2. **Use Compatibility:** The proposed conditional use shall be compatible with the character of the site, adjacent properties, and existing development within the vicinity of the site where the use will be located. In determining compatibility, the Planning Commission shall consider:
- a. Whether the street or other means of access to the site where the proposed conditional use will be located will provide access to the site without materially degrading the service level on such street or any adjacent street;
 - b. Whether the type of use and its location will create unusual pedestrian or vehicle traffic patterns or volumes that would not be expected with the development of a permitted use, based on:
 - i. Orientation of driveways and whether they direct traffic to major or local streets, and, if directed to local streets, the impact on the safety, purpose, and character of these streets;
 - ii. Parking area locations and size, and whether parking plans are likely to encourage street side parking for the proposed use which will adversely impact the reasonable use of adjacent property;
 - iii. Hours of peak traffic to the proposed use and whether such traffic will unreasonably impair the use and enjoyment of adjacent property; and
 - iv. Hours of operation of the proposed use as compared with the hours of activity/operation of other nearby uses and whether the use, during hours of operation, will be likely to create noise, light, or other nuisances that unreasonably impair the use and enjoyment of adjacent property;
 - c. Whether the internal circulation system of any development associated with the proposed use will be designed to mitigate adverse impacts on adjacent property from motorized, non-motorized, and pedestrian traffic;
 - d. Whether existing or proposed utility and public services will be adequate to support the proposed use at normal service levels and will be designed in a manner to avoid adverse impacts on adjacent land uses, public services, and utility resources;
 - e. Whether appropriate buffering or other mitigation measures, such as, but not limited to, landscaping, setbacks, building location, sound attenuation, odor control, will be provided to protect adjacent land uses from excessive light, noise, odor and visual impacts and other unusual disturbances from trash collection, deliveries, and mechanical equipment resulting from the proposed use; and
 - f. Whether detrimental concentration of existing non-conforming or conditional uses substantially similar to the use proposed is likely to occur, based on an inventory of uses within one-quarter (1/4) mile of the exterior boundary of the subject property.

Finding: Because 600 South is a State highway, the number of curb cuts is limited and must be approved by UDOT. The petitioner has chosen to design a project with a single curb cut on 600 South to direct all traffic on the site. Parking is separated from general traffic by having its own entry from 200 West. The layout of the site places creates a central avenue where the hotels will front. The hours of operation are consistent with other uses on the street.

600 South is capable of handling traffic generated by this proposal. The Transportation Division has reviewed the internal circulation for the proposal and has not raised significant concern regarding street capacity. They expressed concern regarding angled stalls on 200 West. Planning Staff shares those concerns regarding the alteration of the public way.

There is no detrimental concentration of Conditional Uses in the vicinity (Attachment D).

3. Design Compatibility: The proposed conditional use shall be compatible with the character of the area where the use will be located with respect to:

- a. Site design and location of parking lots, access ways, and delivery areas;
- b. Whether the proposed use, or development associated with the use, will result in loss of privacy, objectionable views of large parking or storage areas; or views or sounds of loading and unloading areas; and
- c. Intensity, size, and scale of development associated with the use as compared to development and uses in the surrounding area.
- d. If a proposed conditional use will result in new construction or substantial remodeling of a commercial or mixed-used development, the design of the premises where the use will be located shall conform to the conditional building and site design review standards set forth in Chapter 21A.59 of this title. *(Separate analysis later in this report)*

Finding: The planned development allows for more efficient use of the site while still maintaining a strong presence on 600 South and 500 South. The surrounding uses are retail and hotel. The development is in scale with surrounding development and the proposed conditional use is compatible. The parking is below ground. All buildings do not have a door fronting the public street, but this is not inconsistent with other hotels in the area and the streetscape is generally less urban than the central core. All buildings do not meet the 5 foot maximum setback as required by the D-1 zoning, but they meet the intent of the ordinance by providing a strong street presence: The Planning Commission may waive this requirement.

4. Detriment to Persons or Property: The proposed conditional use shall not, under the circumstances of the particular case and any conditions imposed, be detrimental to the health, safety, and general welfare of persons, nor be injurious to property and improvements in the community, existing surrounding uses, buildings, and structures. The proposed use shall:

- a. Not emit any known pollutant into the ground or air that will detrimentally affect the subject property or any adjacent property;
- b. Not encroach on any river or stream, or direct runoff into a river or stream;
- c. Not introduce any hazard or potential for damage to an adjacent property that cannot be mitigated;
- d. Be consistent with the type of existing uses surrounding the subject property; and
- e. Improve the character of the area by encouraging reinvestment and upgrading of surrounding properties.

Finding: The Conditional Use does not emit any pollutants or impact any environmentally fragile sites, nor is it adjacent to any rivers or streams. The Conditional Use encourages the location of the parking in a centralized underground lot.

5. Compliance with Other Applicable Regulations: The proposed conditional use and any associated development shall comply with any other applicable code or ordinance requirement.

Finding: With the exception of modifications to the Zoning Ordinance standards approved by the Planning Commission, all applicable city Code requirements must be met..

21A.54.150 E Additional Standards for Planned Developments

1. **Minimum Area:** A planned development proposed for any parcel or tract of land under single ownership or control shall have a minimum net lot area for each zoning district as set forth in table [21A.54.150E2](#) of this section.

Finding: The minimum lot size is 20,000 square feet. This lot is over 157,000 square feet and meets this standard.

2. **Density Limitations:** Residential planned developments shall not exceed the density limitation of the zoning district where the planned development is proposed. The calculation of planned development density may include open space that is provided as an amenity to the planned development. Public or private roadways located within or adjacent to a planned development shall not be included in the planned development area for the purpose of calculating density.

Finding: This project is not residential . This standard does not apply

3. **Consideration Of Reduced Width Public Street Dedication:** A residential planned development application may include a request to dedicate the street to Salt Lake City for perpetual use by the public. The request will be reviewed and evaluated individually by appropriate departments, including transportation, engineering, public utilities, public services and fire. Each department reviewer will consider the adequacy of the design and physical improvements proposed by the developer and will make recommendation for approval or describe required changes. A synopsis will be incorporated into the staff report for review and decision by the Planning Commission. Notwithstanding the foregoing, no such street will be accepted as a publicly owned street unless there is a minimum width of twenty feet (20') of pavement with an additional right-of-way as determined by the Planning Commission.

Finding: This project does not include dedication of a new public street .This standard does not apply

4. **Planned Developments:** Planned developments within the TC-75, RB, R-MU, MU, CN, CB, and CSHBD zoning districts and the South State Street Overlay. Also planned developments within the CS zoning district, when the district is adjacent to more than sixty percent (60%) residential zoning (within 300 feet, either on the same block or across the street).

Planned developments within these zoning districts may be approved subject to consideration of the following general conceptual guidelines (a positive finding for each is not required):

- a. The development shall be primarily oriented to the street, not an interior courtyard or parking lot,
- b. The primary access shall be oriented to the pedestrian and mass transit,
- c. The facade shall maintain detailing and glass in sufficient quantities to facilitate pedestrian interest and interaction,
- d. Architectural detailing shall emphasize the pedestrian level of the building,

- e. Parking lots shall be appropriately screened and landscaped to minimize their impact on the neighborhood,
- f. Parking lot lighting shall be shielded to eliminate excessive glare or light into adjacent neighborhoods,
- g. Dumpsters and loading docks shall be appropriately screened or located within the structure, and
- h. Signage shall emphasize the pedestrian/mass transit orientation.

Finding: This project is not in any of the aforementioned zoning districts. This standard does not apply

5. Perimeter Setback: The perimeter side and rear yard building setback shall be the greater of the required setbacks of the lot or adjoining lot, unless modified by the Planning Commission.

Finding: The adjacent zoning districts are identical and do not require side yard setbacks. This standard does not apply.

6. Topographic Change: The Planning Commission may increase or decrease the side or rear yard setback where there is a topographic change between lots.

Finding: This project does not have a significant topographic change. This standard does not apply.

Additional analysis

21A.59.060 Standards For Design Review:

In addition to standards provided in other sections of this title for specific types of approval, the following standards shall be applied to all applications for design review:

- A. Development shall be primarily oriented to the street, not an interior courtyard or parking lot.
 - 1. Primary building orientation shall be toward the street rather than the parking area. The principal entrance shall be designed to be readily apparent.
 - 2. At least sixty percent (60%) of the street frontage of a lot shall have any new building located within ten feet (10') of the front setback. Parking is permitted in this area.
 - 3. Any buildings open to the public and located within thirty feet (30') of a public street shall have an entrance for pedestrians from the street to the building interior. This entrance shall be designed to be a distinctive and prominent element of the building's architectural design, and shall be open to the public during all business hours.
 - 4. Each building shall incorporate lighting and changes in mass, surface, or finish to give emphasis to its entrances.

Finding: The buildings would be required to have pedestrian entrances onto 600 South to meet the intent of this requirement. The developer is proposing doors facing the central drive. Given the auto orientation of 600 South this is acceptable as long as the windows facing 600 South remain unobstructed; providing visual access to the interior of the building.

- B. Primary access shall be oriented to the pedestrian and mass transit.
 - 1. Each building shall include an arcade, roof, alcove, portico, awnings, or similar architectural features that protect pedestrians from the rain and sun.

Finding: Architectural fenestration is proposed as part of the development. The sidewalk in front of the hotel building should be extended to connect to the sidewalk along 600 South to accommodate any pedestrian traffic along the street.

C. Building facades shall include detailing and glass in sufficient quantities to facilitate pedestrian interest and interaction.

1. At least forty percent (40%) of any first floor wall area that faces and is within thirty feet (30') of a primary street, plaza, or other public open space shall contain display areas, windows, or doorways. Windows shall allow views into a working area or lobby, a pedestrian entrance, or display area. First floor walls facing a side street shall contain at least twenty five percent (25%) of the wall space in window, display area, or doors. Monolithic walls located within thirty feet (30') of a public street are prohibited.
2. Recessed or projecting balconies, verandas, or other usable space above the ground level on existing and new buildings is encouraged on a street facing elevation. Balconies may project over a public right of way, subject to an encroachment agreement issued by the city.

Finding: The proposed retail buildings meets the requirement for minimum glass facing 600 South. This glass must remain unobstructed.

D. Architectural detailing shall emphasize the pedestrian level of the building.

Finding: The proposed complex is a multiple story development with the major entrances at pedestrian level. There is a sky-bridge across private property connecting the conference levels of two hotels.

E. Parking lots shall be appropriately screened and landscaped to minimize their impact on adjacent neighborhoods.

1. Parking areas shall be located behind or at one side of a building. Parking may not be located between a building and a public street.
2. Parking areas shall be shaded by large broadleaf canopied trees placed at a rate of one tree for each six (6) parking spaces. Parking shall be adequately screened and buffered from adjacent uses.
3. Parking lots with fifteen (15) spaces or more shall be divided by landscaped areas including a walkway at least ten feet (10') in width or by buildings.

Finding: The proposed parking lot is below ground with the exception of limited parking along the central corridor and surface parking for the existing (not part of this proposal) hotel. The plan meets the general landscaping requirement. The final landscape plan should be developed to insure compliance with the details of this standard; including landscaping in the public right-of-way

F. Parking lot lighting shall be shielded to eliminate excessive glare or light into adjacent neighborhoods.

Finding: There is no adjacent residential neighborhood. Lighting levels shall be reviewed prior to issuance of a building permit.

G. Parking and on site circulation shall be provided.

1. Connections shall be made when feasible to any streets adjacent to the subject property and to any pedestrian facilities that connect with the property.
2. A pedestrian access diagram that shows pedestrian paths on the site that connect with a public sidewalk shall be submitted.

Finding: Parking and on-site circulation should be provided to the satisfaction of the Salt Lake City Transportation Department. The retail sidewalk must be extended to meet the City sidewalk.

H. Dumpsters and loading docks shall be appropriately screened or located within the structure.

1. Trash storage areas, mechanical equipment, and similar areas are not permitted to be visible from the street nor permitted between the building and the street.
2. Appropriate sound attenuation shall occur on mechanical units at the exterior of buildings to mitigate noise that may adversely impact adjacent residential uses.

Finding: There are no adjacent residential areas. All major loading is to the rear of the site or underground, not visible from the street. Final plans will receive more detailed review for compliance prior to the issuance of a building permit

I. Signage shall emphasize the pedestrian/mass transit orientation.

Finding: Signage is generally located above the first story retail space, which is generally at pedestrian level..

J. Lighting shall meet the lighting levels and design requirements set forth in chapter 4 of the Salt Lake City lighting master plan dated May 2006.

Finding: Lighting levels will be reviewed prior to the issuance of a building permit

K. Streetscape improvements shall be provided as follows:

1. One street tree chosen from the street tree list shall be placed for each thirty feet (30') of property frontage on a street.
2. Landscaping material shall be selected that will assure eighty percent (80%) ground coverage occurs within three (3) years.
3. Hardscape (paving material) shall be utilized to designate public spaces. Permitted materials include unit masonry, scored and colored concrete, grasscrete, or combinations of the above.
4. Outdoor storage areas shall be screened from view from adjacent public rights of way. Loading facilities shall be screened and buffered when adjacent to residentially zoned land and any public street.
5. Landscaping design shall include a variety of deciduous and/or evergreen trees, and shrubs and flowering plant species well adapted to the local climate.

Finding: Compliance will be determined prior to the issuance of a building permit.

L. Street trees shall be provided as follows:

1. Any development fronting on a public or private street shall include street trees planted consistent with the city's urban forestry guidelines and with the approval of the city's urban forester.
2. Existing street trees removed as the result of a development project shall be replaced by the developer with trees approved by the city's urban forester.

Finding: Compliance will be determined prior to the issuance of a building permit.

M. The following additional standards shall apply to any large scale developments with a gross floor area exceeding sixty thousand (60,000) square feet:

1. The orientation and scale of the development shall conform to the following requirements:

- a. Large building masses shall be divided into heights and sizes that relate to human scale by incorporating changes in building mass or direction, sheltering roofs, a distinct pattern of divisions on surfaces, windows, trees, and small scale lighting.
- b. No new buildings or contiguous groups of buildings shall exceed a combined contiguous building length of three hundred feet (300').
- 2. Public spaces shall be provided as follows:
 - a. One square foot of plaza, park, or public space shall be required for every ten (10) square feet of gross building floor area.
 - b. Plazas or public spaces shall incorporate at least three (3) of the five (5) following elements:
 - i. Sitting space of at least one sitting space for each two hundred fifty (250) square feet shall be included in the plaza. Seating shall be a minimum of sixteen inches (16") in height and thirty inches (30") in width. Ledge benches shall have a minimum depth of thirty inches (30");
 - ii. A mixture of areas that provide shade;
 - iii. Trees in proportion to the space at a minimum of one tree per eight hundred (800) square feet, at least two inch (2") caliper when planted;
 - iv. Water features or public art; and/or
 - v. Outdoor eating areas or food vendors.

Finding: The proposed building contains significant architectural detailing to minimize their scale. The purpose of the planned development is to allow multiple building instead of a single large building. Details of landscaping will be reviewed prior to the issuance of a building permit.

N. Any new development shall comply with the intent of the purpose statement of the zoning district and specific design regulations found within the zoning district in which the project is located as well as adopted master plan policies, the city's adopted "Urban Design Element" and design guidelines governing the specific area of the proposed development. Where there is a conflict between the standards found in this section and other adopted plans and regulations, the more restrictive regulations shall control.

Finding: The proposed buildings generally meet the Downtown Master Plan and the Urban Design Element by providing density adjacent to the urban core, providing variety in the skyline and developing buildings of varying heights that are shorter than the core, but taller than the fringe area of downtown..

21A.59.020 Authority:

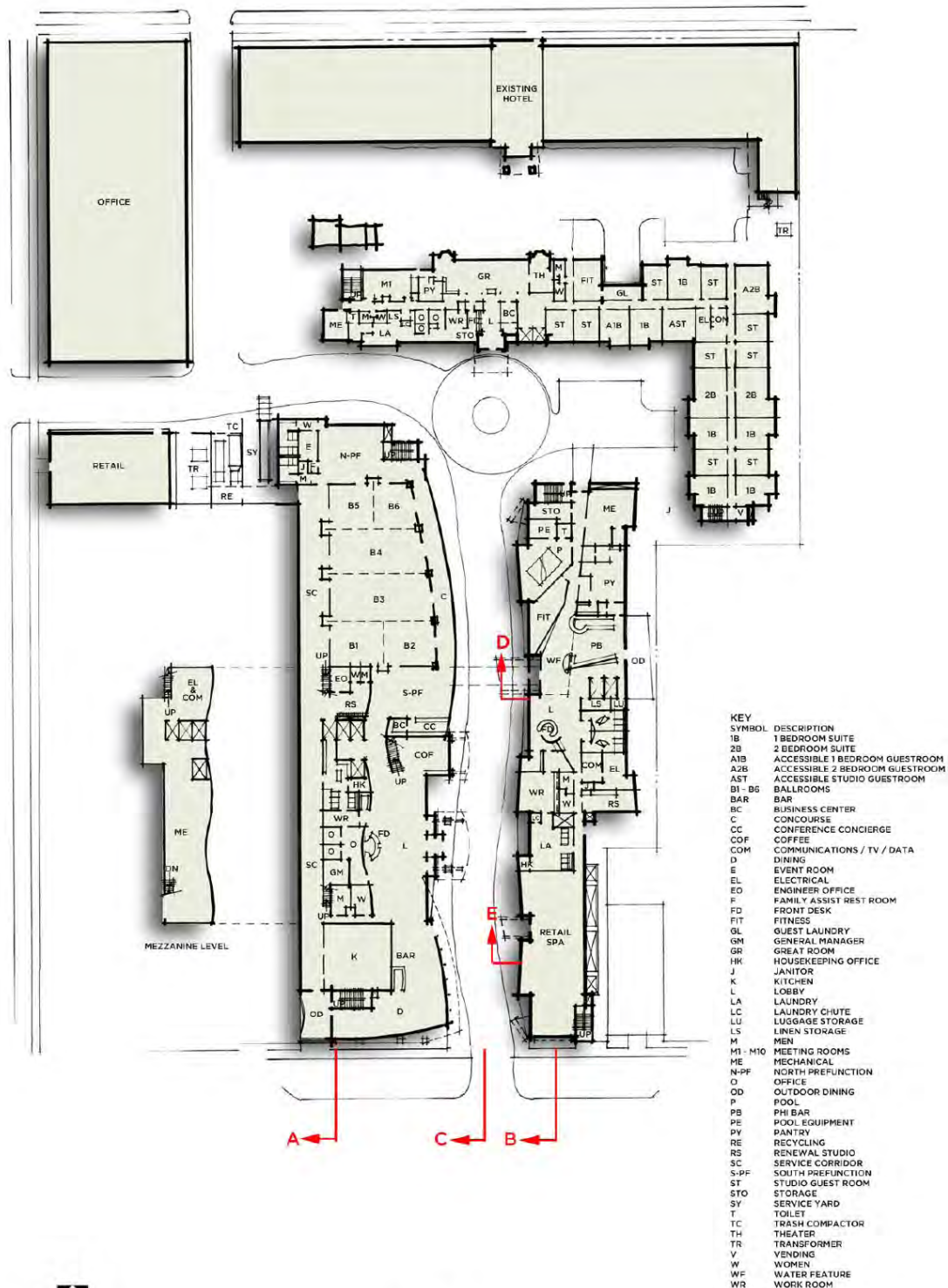
Design review shall be required pursuant to the provisions of this chapter for uses as specified within individual zoning districts before zoning certificates, building permits or certificates of occupancy may be issued.

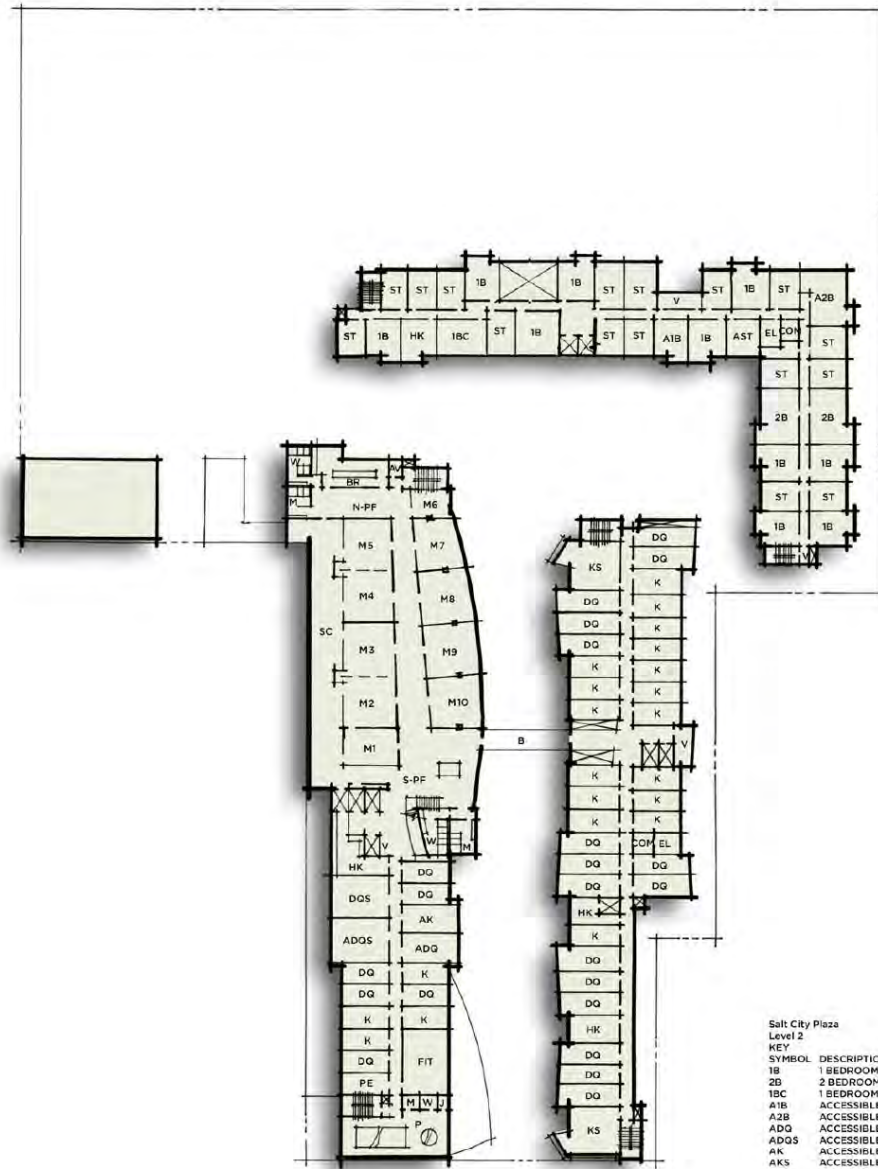
A. The planning commission shall approve design criteria upon consideration of comments received from city departments and determining whether modification of specific design regulations meets the intent of the individual zoning district.

B. The planning commission may modify individual design requirements for specific projects if they find that the intent of the basic design criteria of the zoning district has been met. (Ord. 3-05 11, 2005

Finding: The project generally meets the intent of the design standards, but does not always meet specific criteria, such as a door facing the street. Given the general development pattern of the neighborhood these exceptions are warranted.

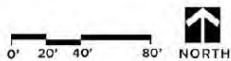
Attachment A
Site Plan and Elevation Drawings





Salt City Plaza
Level 2

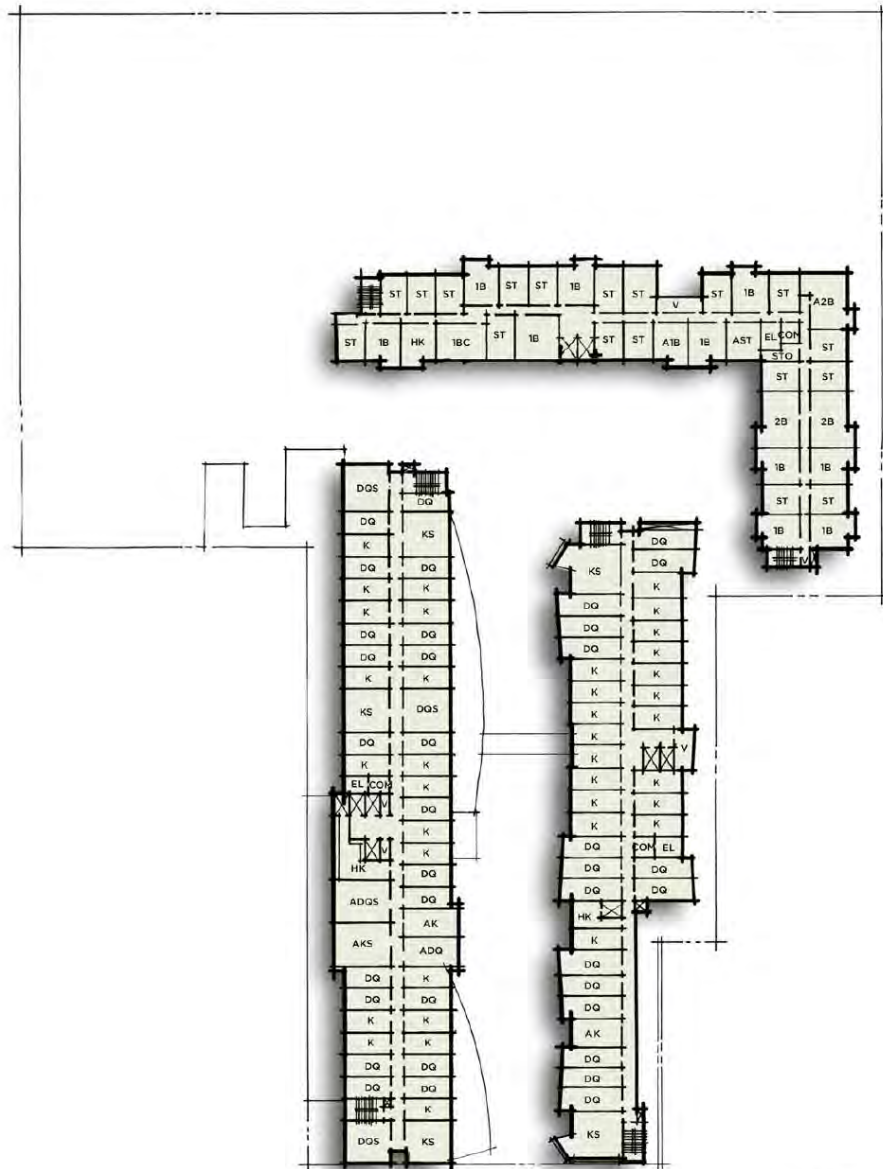
KEY	DESCRIPTION
1B	1 BEDROOM SUITE
2B	2 BEDROOM SUITE
1BC	1 BEDROOM CONF. SUITE
A1B	ACCESSIBLE 1 BEDROOM GUESTROOM
A2B	ACCESSIBLE 2 BEDROOM GUESTROOM
ADQ	ACCESSIBLE DOUBLE QUEEN
ADQS	ACCESSIBLE DOUBLE QUEEN SUITE
AK	ACCESSIBLE KING
AKS	ACCESSIBLE KING SUITE
AST	ACCESSIBLE STUDIO GUESTROOM
AV	AUDIO / VISUAL
B	BRIDGE
BR	BOARD ROOM
C	CONCOURSE
COM	COMMUNICATIONS / TV / DATA
DQ	DOUBLE QUEEN GUESTROOM
DQS	DOUBLE QUEEN GUESTROOM SUITE
EL	ELECTRICAL
FIT	FITNESS
HK	HOUSEKEEPING
J	JANITOR
K	KING GUESTROOM
KS	KING SUITE GUESTROOM
M	MEN
M1 - M10	MEETING ROOMS
ME	MECHANICAL
N-PF	NORTH PREFUNCTION
P	POOL
PE	POOL EQUIPMENT
SC	SERVICE CORRIDOR
S-PF	SOUTH PREFUNCTION
ST	STUDIO GUEST ROOM
STO	STORAGE
T	TOILET
TR	TRANSFORMER
V	VENDING
W	WOMEN



Level 2

Salt City Plaza
Salt Lake City, Utah





Salt City Plaza
Level 3

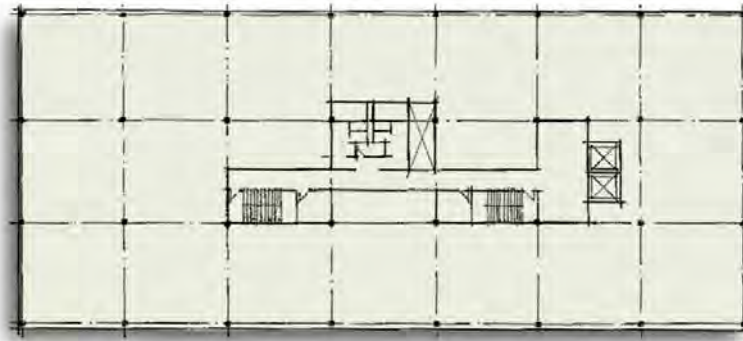
KEY	DESCRIPTION
1B	1 BEDROOM SUITE
1BC	1 BEDROOM CONF. SUITE
2B	2 BEDROOM SUITE
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ADQS	ACCESSIBLE DOUBLE QUEEN SUITE
AK	ACCESSIBLE KING
AKS	ACCESSIBLE KING SUITE
AST	ACCESSIBLE STUDIO GUESTROOM
COM	COMMUNICATIONS / TV / DATA
DQ	DOUBLE QUEEN GUESTROOM
DQS	DOUBLE QUEEN GUESTROOM SUITE
EL	ELECTRICAL
HK	HOUSEKEEPING
K	KING GUESTROOM
KS	KING SUITE GUESTROOM
ST	STUDIO
STO	STORAGE
V	VENDING
W	WOMEN

0' 20' 40' 80' NORTH

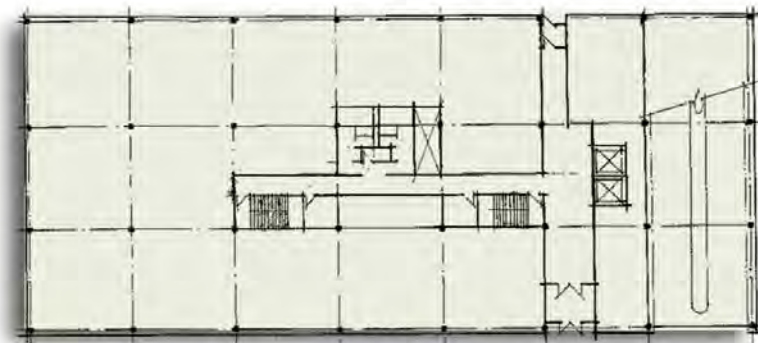
Level 3

Salt City Plaza
Salt Lake City, Utah

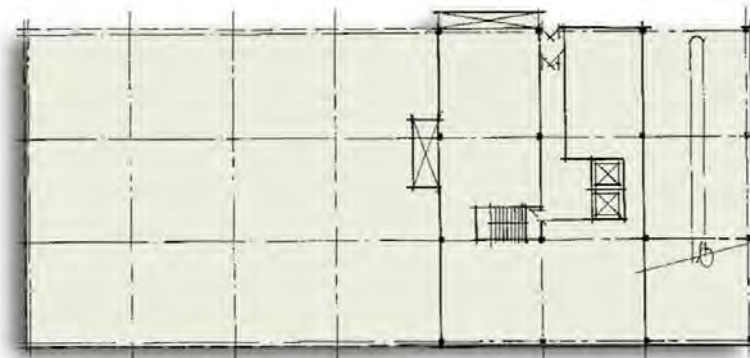
GSBS
ARCHITECTS



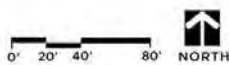
Office Building Upper Levels



Office Building Level 1



Office Building Lower Level





Holiday Inn South Elevation Contemporary



Holiday Inn North Elevation Contemporary



Holiday Inn East Elevation Contemporary

Salt City Plaza

Salt Lake City, Utah



Indigo North Elevation Prototype



Indigo South Elevation Prototype



Indigo West Elevation Prototype

Salt City Plaza

Salt Lake City, Utah





Staybridge West Elevation



Staybridge South Elevation

Salt City Plaza

Salt Lake City, Utah

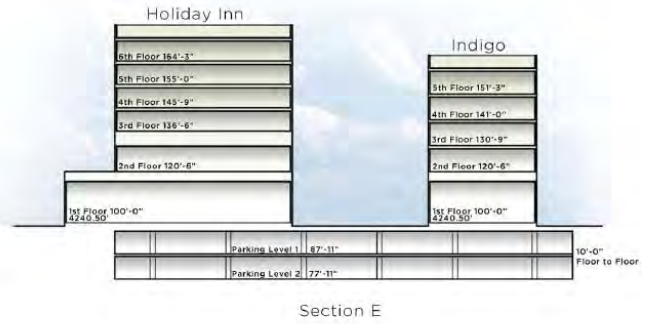
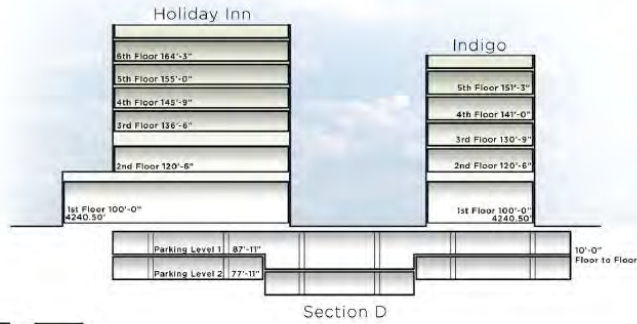
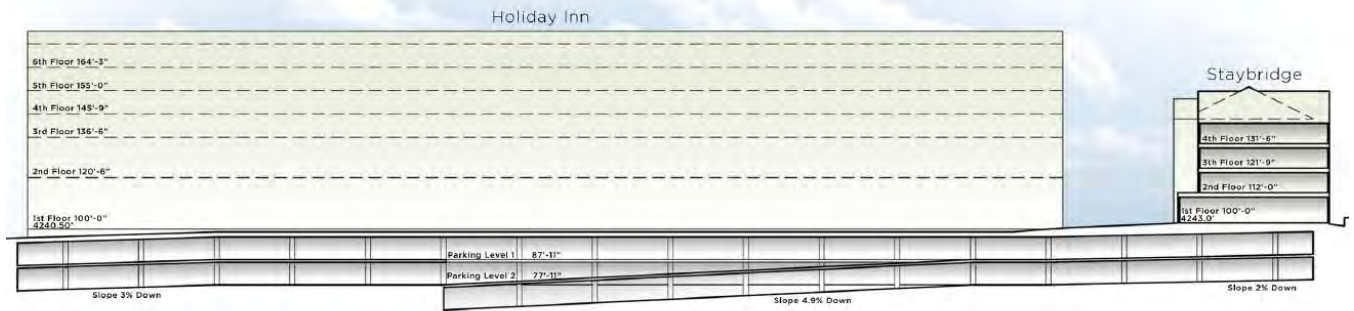


0' 20' 40' 80'

Salt City Plaza

Salt Lake City, Utah

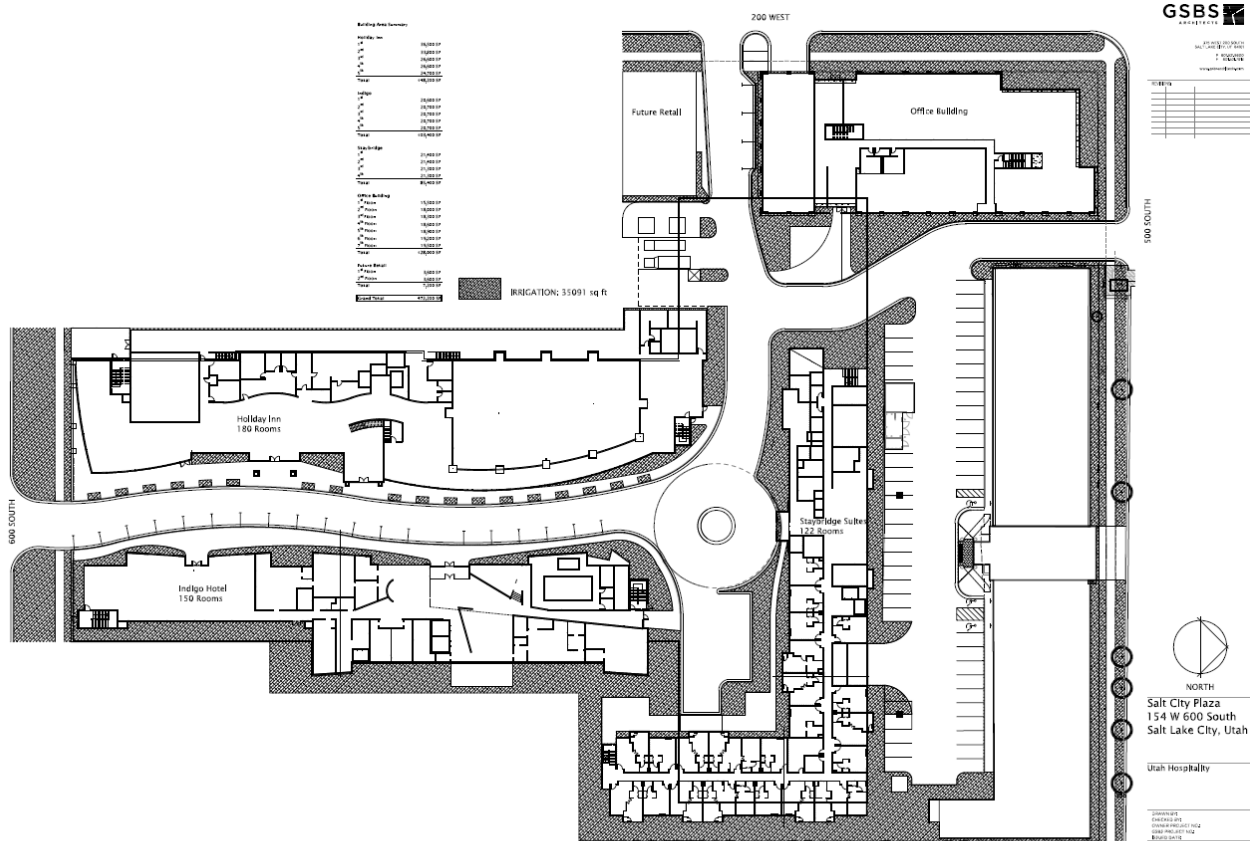




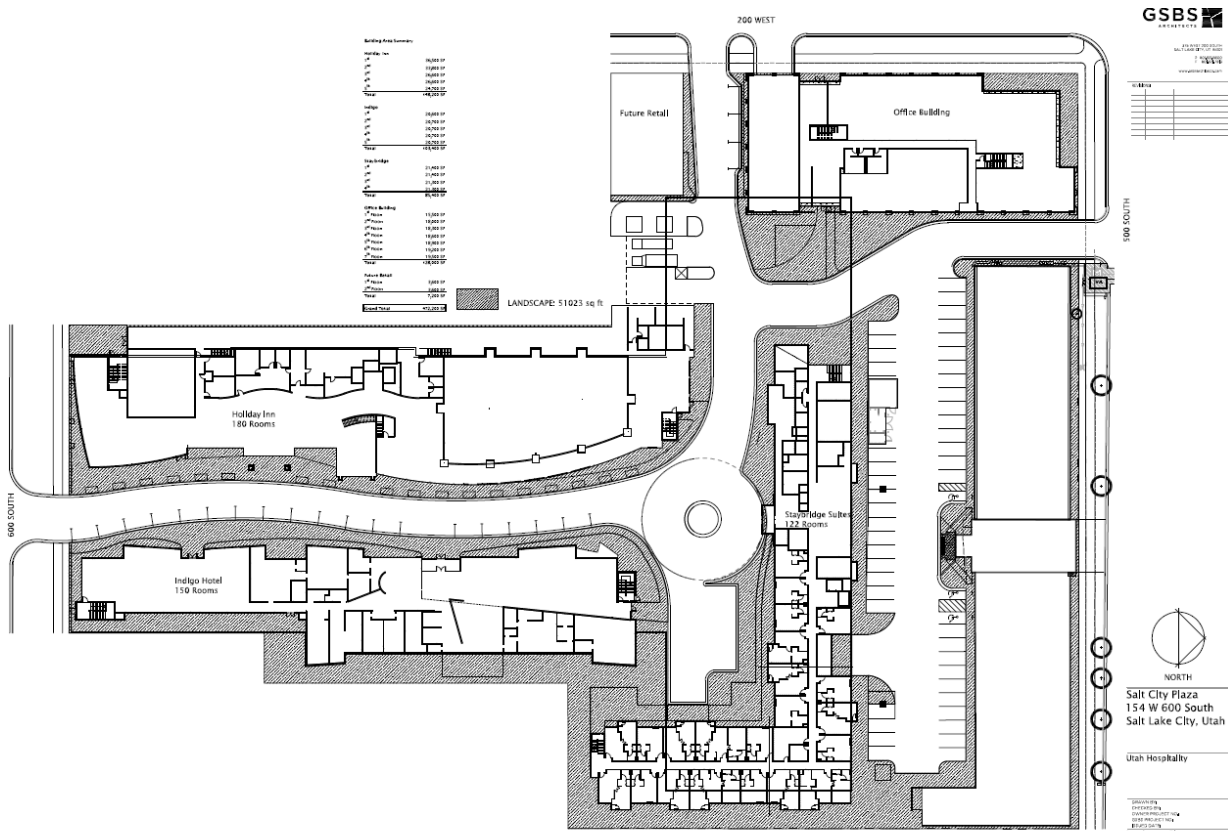
0' 20' 40' 80'

Salt City Plaza
Salt Lake City, Utah

GSBS ARCHITECTS



L1.1



L1.2

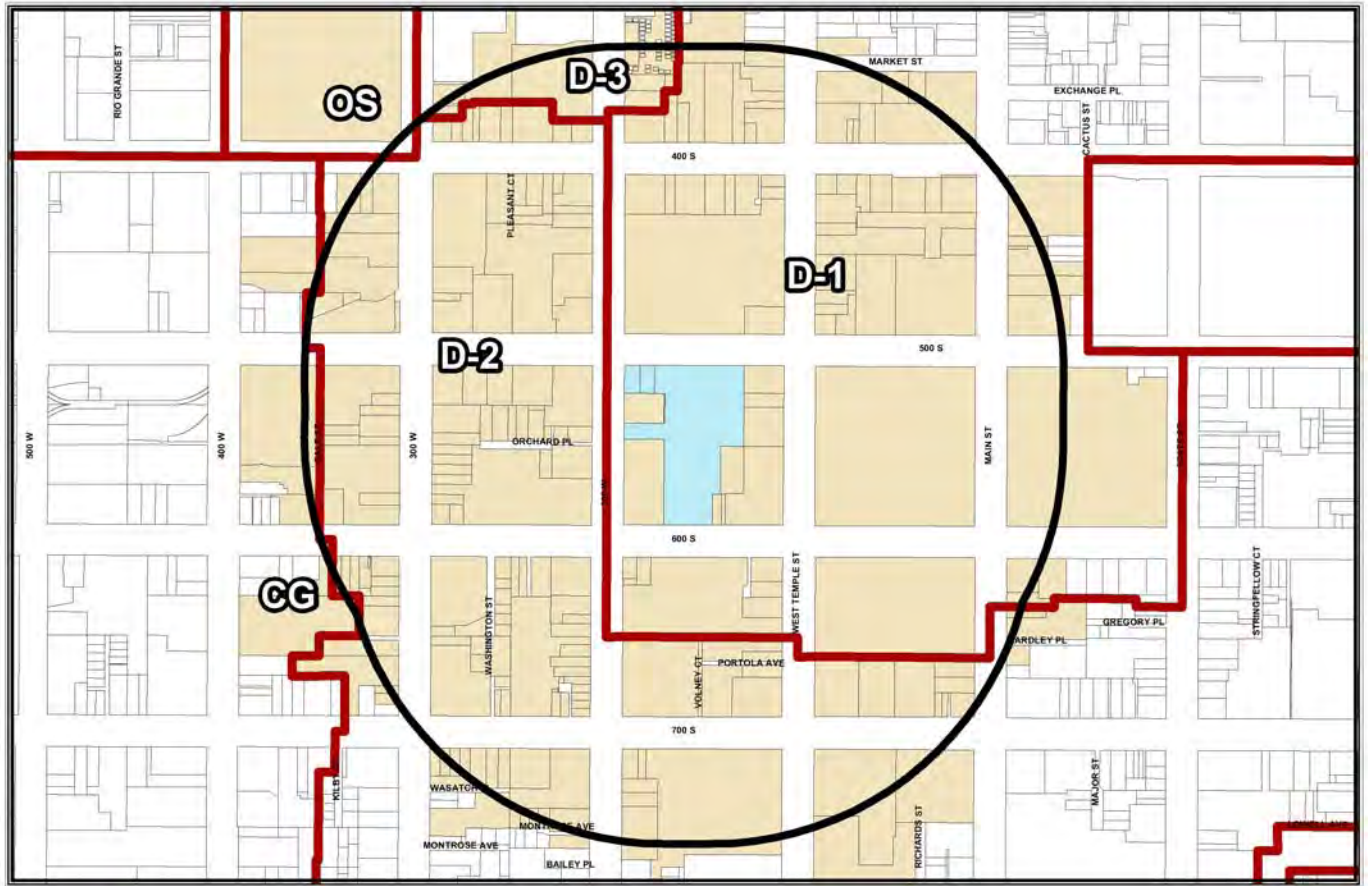
Attachment B
Photographs







Attachment C
Conditional Use Map



- Conditional Uses in Residential Districts
- 3-4 Dwelling Units NonConforming Uses
- Multi-Family NonConforming Uses
- Commercial/Office NonConforming Uses

■ Subject Properties

○ 1320 Foot Buffer around the Subject Property

■ Parcels that Intersect the 1320 Foot Buffer around the Subject Property



Attachment D

Planning Commission Subcommittee notes

Attendees:

Planning Commission: Mary Woodhead, Mathew Wirthlin, and Susie McHugh

Planning Division Staff: Doug Dansie and Joel Paterson

Applicant: Salt City Plaza

Background and Project Location: 200 West 500 South

Presentation in summary including changes to the project: The applicant is proposing a Planned Unit Development/Conditional Use. Garden Inn, Holiday Inn, and Indigo are all owned by State Bridge owners. Indigo will have a European, yet contemporary upbeat flare. The applicant stated that there would be three options for someone who wanted to stay in a hotel in the area, this project would offer three different prices and ambiances.

There would also be a conference center built in the Holiday Inn and the development would include an office building with work spaces, 600 parking stalls would be underneath the site. The applicant noted that the site would be able to hold 500 people for an event.

Staff/Subcommittee recommendation(s), comments and concerns:

Mr. Dansie noted that this petition has been routed to city departments; none of them had any real issues with it.

Commissioner Wirthlin inquired about movement onsite.

The applicant stated that there would be temporary parking on street level, to allow for easier check-in and then patrons would move to the underground parking structure. The main entry would be off of 600 South.

Commissioner Woodhead noted that there did not seem to be much ingress/egress from the street.

The applicant noted that there would be sidewalks on both sides of the project for pedestrians, which would make the project walkable without having to walk through parking lots, so they were planning on people coming into the project, parking, and walking the rest of the time.

Commissioner Wirthlin inquired if there was a setback required for the Holiday Inn building.

Mr. Dansie noted that in a D-1 zone, no more than five (5) feet from the property line was allowed for a setback.

Commissioners inquired if a lighting plan was required.

The applicant noted that they would provide areas of lighting on the street so people would feel safe walking around the project at night.

Commissioners stated that the lighting should be adequate for safety, but not cause light pollution.

Commissioner Woodhead inquired about the project signage.

The applicant stated that good exposure to signage was near the front doors and at the entry way.

Commissioner McHugh inquired if there would be an airport shuttle provided for patrons, and if a general shuttle would be provided to access the downtown area.

The applicant noted that they would probably provide that.

Commissioner Wirthlin inquired why there was a skybridge between hotels.

The applicant noted that it made it more convenient to access the conference room and restaurants, day spa, and retail space located on the south side of the project, from any of the hotels.

Conclusion:

- **Commissioners agreed that this should be reviewed by the Downtown Community Council and People's Freeway.**
- **Applicant should have a better understanding of pedestrian circulation and lighting on site for the public hearing.**